

Local Agency Program Update from MDOT

Michigan Municipal League
Webinar
August 25, 2021

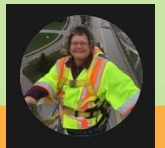
Ryan Doyle, P.E.
MDOT LAP Safety & Urban Manager

Kelly Crannell, P.E.
MDOT Local Agency Construction Engineer



Local Agency Program Vision:

MDOT will assist local agencies with preserving state and federal aid in a fair and consistent manner and serve as an information resource.



MDOT Home Contact Organization Sitemap MI.gov



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Transportation Alternative Program
Safety Program
Passenger Transportation
Aeronautics
Title VI Nondiscrimination
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MDOT / DOING BUSINESS / LOCAL AGENCY PROGRAM

Local Agency Program
 FY 2022 Project Planning Guide
 FY 2021 Project Planning Guide
 Fiscal Year Obligation Reports
 Annual Local Agency Force Account Report

News and Policy Changes
 Local Bridge Program Public Meeting Announcements
 Local Agency Program (LAP) Advisories

Guidance Documents
 National Environmental Policy Act (NEPA) Guidance and Information
 Real Estate Guidance and Information

FHWA Emergency Relief

LAP Podcasts & Presentations

Contact Us

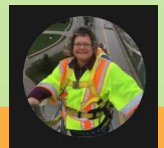
Quick Links

- Title VI
- Nondiscrimination
- Tribal Governments
- Twitter Facebook YouTube
- Mi Drive
- State Map

LAP Links

- LAP Construction
- LPA Stakeholder Partnering
- RTF Program

LAP Web Site –
www.michigan.gov/mdotlap



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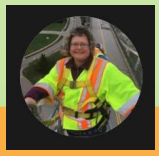
LAP Links
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 • RTF Program

Sign up for email from MDOT!

LAP Construction Link or shortcut

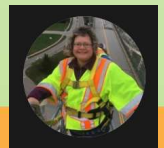
www.michigan.gov/mdotlocalconstruction

LISTSERV Registration Bottom left area of any MDOT page





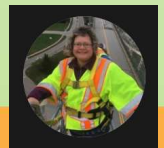
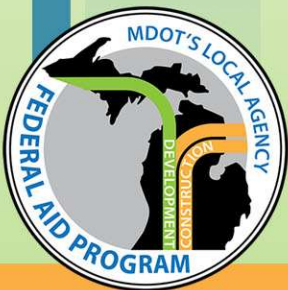
Department Updates



2020 MDOT Spec Book



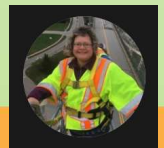
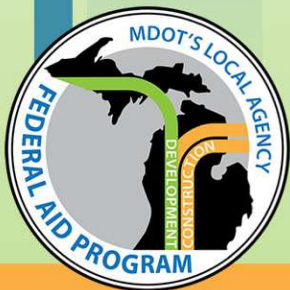
- [MDOT - Plans & Specifications \(michigan.gov\)](http://michigan.gov)
- Website includes 'significant changes' summary
- Implementation NOW (i.e. August 2021 Letting)
- Hard copy in production
- Spec Book Webinars thru LTAP – August 3rd & 5th



LAP Web Site – www.michigan.gov/mdotlap

- Select from the List
- MDOT LAP: MDOT 2020 Standard Specifications for Construction Webinar-August 5, 2021
 - MDOT LAP: MDOT 2020 Standard Specifications for Construction Webinar-August 3, 2021
 - MDOT LAP: Threatened & Endangered Species Webinar Useful Links
 - MDOT LAP: Threatened & Endangered Species Webinar
 - MDOT LAP: Threatened & Endangered Species Webinar Presentation Document
 - LAP Threatened & Endangered Species Announcement and Webinar
 - 2017 Local Agency Program Unique Special Provisions
 - Sweating the Details - Part 4: Guidelines for Geometrics Update
 - MDOT/SHPO Section 106
 - Addressing Risk in Design: Sweating the Details
 - MDOT & USFWS T&E Species I
 - Sweating the Details - Part 3 - Increasing Your Funding Odds
 - Sweating the Details - Part 7
 - Addressing Risk in Design - Sweating the Details Part 2
 - MDOT LAP ROW Webinar
 - LAP NEPA Documentation Announcement and Webinar

- August 3rd Webinar
 - Administrative/Contractual
 - Divisions 1, 8 & 9
- August 5th Webinar
 - Technical/Materials
 - Divisions 2, 3, 4, 5, 6, 7, & 10

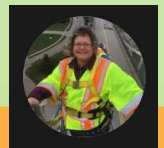
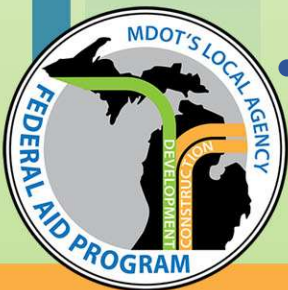


AASHTOWare Project Construction & Materials™ Advisory



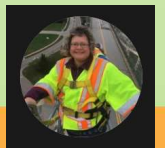
Transition Update

- MDOT trunkline
 - pilots ongoing
 - anticipate full transition by Oct '22 Letting
- Local Agency Program
 - Pilots pending spring 2022
 - Anticipate full transition by Oct '23 Letting
 - possibly late 2020 or 2021
- Field Manager Help desk – 1-800-4MI-MDOT
- Local Let solution – pending thru CRA subcommittee



WORK ZONE SAFETY

— TASK FORCE —



Michigan hosted National Work Zone Awareness Week

2021 National Work Zone Awareness Week a success

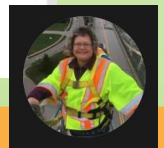
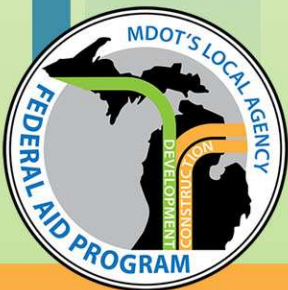


On Tuesday, April 27, MDOT hosted the 2021 National Work Zone Awareness Week (NWZAW) kickoff in Macomb County. With nearly 1,070 views, the [livestream event](#) featured Gov. Gretchen Whitmer, Director Ajegba, and Col. Jasper, and several others who spoke about work zone safety and the need to slow down and pay attention. In emotional testimony, viewers also heard from Leslie Fonzi-Lynch about the tragic death of her son, Brandyn Spychalski, who was hit in 2017 and died due to his injuries in Jan. 2021.

On Wednesday, April 28, MDOT's Bay Region partnered with MSP's Bay Region to step up patrols in the I-75/M-46 work zone. Troopers reported 49 stops, 35 speed citations, and 15 warnings. All drivers who were stopped received information about work zone safety. MSP said that additional patrols are planned.

On Friday, April 30, MDOT and the Ohio Department of Transportation joined forces to hold an additional #NWZAW media event to remind Michiganders and Ohioans that work zones do not end at the state line and to drive safely. In 2020, Michigan and Ohio had more than 9,500 work zone crashes combined.

Thank you to all who helped with and have supported MDOT's work zone safety initiatives!





[Work Zone Safety LINK](#)



Talking Michigan Transportation podcast: Gov. Gretchen Whitmer talks work zone safety and Rebuilding Michigan, MDOT Communications Director Jeff Cranson is joined by maintenance supervisor Steve Wessels to discuss worker safety

On [last week's edition of the Talking Michigan Transportation podcast](#), a discussion about a new [video](#) emphasizing the need for drivers to slow down when driving through road construction projects. Gov. Gretchen Whitmer talks about MDOT's "drive like you work here" message and why it is important as more major roads will be rebuilt in 2021. The governor explained her Rebuilding Michigan plan during the Jan. 31 [edition](#) of the podcast.

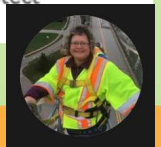
OCTOBER 22, 2020 MICHIGAN DEPARTMENT OF TRANSPORTATION



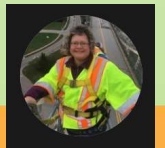
As a Sept. 25 [news release](#) explained, while there are fewer motorists on the roads, the number of road and work zone fatalities are on the rise. The increase in crashes and fatalities comes at a time when the department is reporting a 20 percent decrease in overall statewide traffic volumes due to the ongoing COVID-19 pandemic.

As of September, 730 people have died on Michigan roads. That's an increase of 58 deaths compared to this time last year. In September, vehicles struck two on-the-job road workers, who died from their injuries.

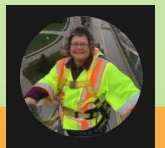
Later, a conversation with Steve Wessels, a veteran maintenance supervisor at MDOT who was featured in a Fox 17 [story](#) talking about efforts to stay safe while working alongside heavy vehicle traffic. Wessels shares his stories from the road and how he trains workers to protect themselves on projects.



Partner Updates

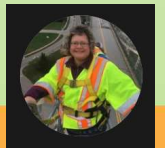


Engineering Updates





U.S. Department of Transportation
**Federal Highway
Administration**



LAP Staffing Updates

Urban Project Development Engineer

- Welcome Michele Zawerucha!
- Started June 7th

NEPA Coordinator

- Welcome Mahreen Nabi!
- Started June 14th



Once up to speed, Michele and Mahreen will be handling NEPA clearance for LAP projects





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 - Bridge Program

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[FY 2022 Project Planning Guide](#)

[FY 2021 Project Planning Guide](#)

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[Local Bridge Program Public Meeting Announcements](#)

[Local Agency Program \(LAP\) Advisories](#)

Guidance Documents

[National Environmental Policy Act \(NEPA\) Guidance and Information](#)

[Real Estate Guidance and Information](#)

**FHWA
Emergency
Relief**

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Presentations**

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- Title VI Nondiscrimination
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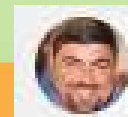
LAP Links

- LAP Construction
- LPA Stakeholder Partnering
- RTF Program

LAP Website

FY 2022 Project Planning Guide

www.Michigan.gov/mdotlap



LAP FY 2022 Planning Guide

Local Agency Programs (LAP) FY 2022 Project Planning Guide May 2022

	SUBMIT NEPA DOCUMENTATION (MDOT FORM 5323) TO LAP, 2-6 MONTHS BEFORE GI SUBMITTAL. DATES SHOWN HERE ARE APPROXIMATELY 4 MONTHS PRIOR TO GI SUBMITTAL DATES	FOR BRIDGE PROJECTS, SUBMIT TYPE, SIZE, AND LOCATION (TS&L) DOCUMENTS TO LAP		SUBMIT ACCEPTABLE GRADE INSPECTION PACKAGE TO LAP	APPROXIMATE GRADE INSPECTION MEETING DATE		SUBMIT FINAL ROW CERTIFICATION (ATTACHMENT B) TO LAP	SUB DOC	
LA begins Project Design, Prepare NEPA Form 5323, and forward to MDOT LAP between 2 and 6 months before GI submittal.									
		03/29/21	M						
		05/03/21	M						
		03/08/21	M	06/01/21	M	05/03/21	M	06/02/21	W
		04/12/21	M	07/06/21	T**	06/07/21	M	07/07/21	W
		05/11/21	M	08/02/21	M	07/06/21	T**	08/04/21	W
		06/07/21	M	08/30/21	M	08/09/21	M	09/08/21	W
		07/06/21	T**	09/27/21	M	09/07/21	T**	10/06/21	W
		08/09/21	M	11/01/21	M	10/04/21	M	11/03/21	W
		09/07/21	T**	11/29/21	M	11/01/21	M	12/01/21	W
Also begin utility notification and coordination, coordination with railroads if applicable, coordination with MDOT for traffic and pedestrian signal work, permit application, property acquisition, etc.									
		08/09/21	M	11/01/21	M	12/06/21	M	01/05/22	W
		09/07/21	T**	11/29/21	M	01/03/22	M	02/02/22	W
		10/04/21	M	12/27/21	M	01/31/22	M	03/02/22	W
		11/08/21	M	01/31/22	M	03/07/22	M	04/06/22	W
		12/06/21	M	02/28/22	M	04/04/22	M	05/04/22	W
		01/10/22	M	04/04/22	M	05/09/22	M	06/08/22	W
		02/07/22	M	05/02/22	M	06/06/22	M	07/06/22	W

Local Agency Program (LAP) Project Planning Guide

The goal of this document is to discuss the project development process for local agency projects let by MDOT. The suggested time frames below can vary due to the differences in complexity of projects; however, the following project life cycle is recommended in order to meet the desired letting date. The schedule does not consider when the federal fund obligational authority may expire during a given fiscal year, which creates the potential for delays and potentially the loss of a project.

Note: This schedule does not apply to special funding programs such as the Emergency Response (ER) program, or to innovative delivery methods such as design-build and fixed-price-variable-scope (FPVS). For potential non-design-bid-build projects, please contact the MDOT LAP Supervisor or the MDOT Innovative Contracting Unit Supervisor.

22+ Weeks Before Desired Letting Date – Local Agency Begins Project Design

- Complete and submit National Environmental Policy Act (NEPA) Requirements Form #5323, available at <https://mdotboss.state.mi.us/webforms/FormDetails.htm?formNumber=5323>. For projects that may require an Environmental Assessment (EA), submit the EA document to LAP at least one year before making the Grade Inspection (GI) submittal.
 - Form #5323 is required for all projects.
 - The timeframe for submittal will be dependent on the complexity of the project and is currently recommended at **2 to 6 months prior to the GI submittal**. The additional time is needed so that MDOT can review and approve the NEPA classification for the project.
 - Submit the completed form #5323 to the appropriate LAP Project Development Engineer (PDE) listed on the form. The PDE will create a ProjectWise (PWZ) folder for the project upon receipt. If additional permissions for folder access are needed, please request these permissions in your submittal.
- Complete Public Stakeholder Involvement- at a minimum, as required by NEPA and project type
- Complete Utility Notification and Coordination
- Begin Property Acquisition (if needed)
- Prepare an application for a Section 106 review and submit the application to the Michigan State Historic Preservation Office (SHPO). Reminder: The application must be completed by an individual/firm that meets the SHPO's qualifications. Also contact representatives of the 12 recognized Tribal Governments if required, request their input in the project, and maintain records of the correspondence. **Because each tribal government is a recognized sovereignty, contacts must be between governmental units. Therefore, the Act 51 Local Agency, which is a governmental unit, must make the tribal contact, and the local agency's consultant or any other designated person or group, shall not make the contact.**
- Complete soil investigation and geotechnical study (required for structures including bridges, boardwalks, and retaining walls; proposed signal poles; and watermain and sewer projects).
- Prepare and submit applications for necessary permits, and coordinate with other agencies and departments (U.S. Army Corps of Engineers, MDOT, other local agencies, etc.)

Please Note:

See the attached guidance use document, "Local Agency Program (LAP) Project Planning Guide"

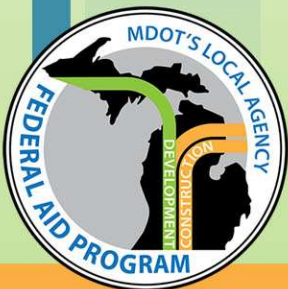
** date adjusted for holiday

Dates shown for NEPA document submittal are generally 4 months before GI submittal. However LAP recommends NEPA submittal 2-6 months before GI submittal

All bridge projects (bridge replacement and major rehabilitation regardless of funding) require a TS&L submittal.

For projects that may require an Environmental Assessment (EA), submit the EA document to LAP at least one year prior to submitting the Program Application.

For projects which FHWA has identified as Projects of Divisional Interest (PODI), submit Plans, Proposal, and Estimate to LAP at least two weeks prior to the dates shown in this guide.



LAP Project Certification (Form #2664)

- Must be submitted with all final submittals on and after October 1st, 2021
- Addresses FHWA concerns raised in their annual audit:
 - Approval of PS&E
 - Railroad Coordination
 - Utility Coordination
- Replaces “request to advertise” and certification email
- Local Agency Responsible Charge and Staff Engineer must E-Sign


The MDOT Staff Engineer will provide this form to the Local Agency and copy the Local Agency's Project Engineer. The form shall be returned to the MDOT Staff Engineer once any final revisions requested by the Staff Engineer have been completed. This form must be certified prior to obligation.

PROJECT INFORMATION		
LOCAL AGENCY NAME		JOB NUMBER(S)
ROUTE		LOCATION
LOCAL PUBLIC AGENCY RESPONSIBLE CHARGE & TITLE		
PHONE NUMBER	E-MAIL ADDRESS	
MDOT LAP STAFF ENGINEER	PHONE NUMBER	E-MAIL ADDRESS



LAP Project Certification (Form #2664)

PROJECT CERTIFICATION

1. I have reviewed and understand the above requirements.
2.  I approve the plans, specifications, and estimate submitted to MDOT, and verify that they are complete and final. In addition, I have reviewed to ensure that these documents continue to adhere to the current MDOT #5323 Form (NEPA Document) submittal.
3. The project meets all requirements in the current Americans with Disabilities Act (ADA).
4. All steel and iron components proposed to be permanently incorporated at the time of design of this project are within the limits of the Buy America requirements as stated in 23 CFR635.410.
5. Railroad work associated with potential conflicts with the planned construction has been completed or all necessary arrangements have been made for the project to be undertaken and completed as detailed in the physical construction schedule. (If it is determined that the completion of such work in advance of the project construction is not feasible, there shall be appropriate notification provided in the bid proposal identifying the railroad work which is to be underway concurrently with the project construction.)
6. Project work and schedule has been communicated and coordinated with all known utilities within the project limits. Known potential utility conflicts have been identified and a conflict resolution strategy has been communicated to impacted utilities. Utility relocations will be completed prior to the project's start date with the exception of work identified in the bid documents. Relocations taking place after the project's start date as well as known utilities requiring contractor coordination, potentially impacting project schedule, are detailed in the bid documents. Documentation is on file to support project utility coordination activities.
7. I understand the requirements for construction engineering and other administrative responsibilities for this project, as outlined in the [Local Agency Program \(LAP\) section of the MDOT Construction Manual](#). I understand, per 23CFR635.105, it is the local agency's responsibility to be adequately staffed and suitably equipped to undertake and satisfactorily complete this work.
8. As Local Public Agency Responsible Charge for this project, I verify that I am a full-time employee of the local agency. I understand that I am the official local agency contact and I am responsible to represent the local agency interests during the project.



LAP Project Certification (Form #2664)

I certify all the above project certification statements to be true and have submitted all required NEPA and ROW documentation to MDOT for review and approval. Once all necessary certifications, permits, and required items are reviewed by MDOT, I request MDOT to obligate and advertise this project.

LOCAL PUBLIC AGENCY RESPONSIBLE CHARGE'S E-SIGNATURE

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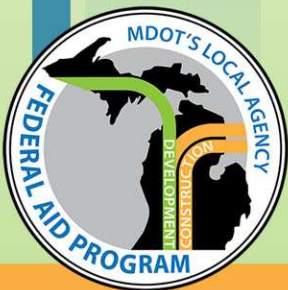
MDOT acknowledges that the local agency has submitted and certified all of the required documents for advertisement.

MDOT LAP STAFF ENGINEER'S E-SIGNATURE

?

Return this form as soon as possible to the staff engineer. Delays in receipt may cause a delay in obligation and advertisement.

- Must be completed prior to submitting for obligation
- Any estimate changes or major spec/plan changes will require an updated Project Certification form



Electronic Transition

100% Digital Submittals!

TRANSITION TO 100% ELECTRONIC / DIGITAL PROJECT FILES & ELECTRONIC SIGNATURES

This advisory updates the previous LAP Advisory dated November 19, 2020.

Effective March 1, 2021, Local Agency Programs (LAP) will generally accept only electronic or digital submittals, project files, and signatures. As an exception to this requirement, LAP will continue to accept plan title sheets and log title sheets that are hand signed and sealed, and then scanned electronically.

LAP cost sharing agreements will continue to be sent to local agencies electronically. Electronic signatures are highly encouraged in order to expedite execution of agreements; however, printed copies with original ink signatures will continue to be accepted for these agreements only.

Please note that signatures must be verified electronic DocuSign signatures. Documents that are hand-signed then scanned are not acceptable after March 1, 2021, except as noted above.

All files shall be emailed to the appropriate LAP contact, or placed into the appropriate ProjectWise folder (PWZ). After uploading the file(s) to PWZ, please notify the proper LAP contact that the files have been uploaded.

- Effective March 1, 2021:
 - All submittals to LAP must be electronic!
 - Exceptions for cost sharing agreements & title sheets
 - ... but electronic signatures are strongly encouraged!
- Only use DocuSign for electronic signatures



DocuSign training videos, software installation instructions, or additional information: <http://www.michigan.gov/MDOT-esign>

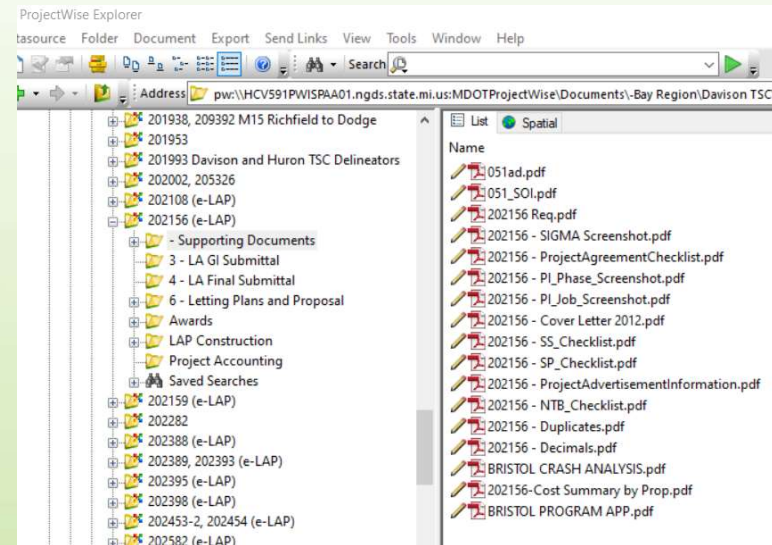


Electronic Transition

THIS INCLUDES:

PREPARED BY: (Signature of P.E.)	DATE	AFFIX LICENSE, REGISTERED PROFESSIONAL ENGINEER SEAL
PREPARED BY: (Signature of P.E.)		AFFIX LICENSE, REGISTERED PROFESSIONAL ENGINEER SEAL
PREPARED BY'S TYPED NAME AND TITLE	PREPARED BY'S REGISTRATION NUMBER	EXPIRATION DATE
PREPARED BY'S E-MAIL ADDRESS	PREPARED BY'S PHONE NUMBER	
ACCEPTED BY LOCAL AGENCY (Signature of Authorized Person Employed by the County, City or Village)	DATE	
TYPED NAME AND TITLE		

E-Signing AND E-SEALING Program Applications



Placing all necessary files in Projectwise



Project Signature Sheet

Contract ID 82024-113552 File Index


Plan Files		
File Name	Description	Notes
113552_Bridge_1.pdf	Bridge Plans	
113552_Bridge_2.pdf	Bridge Plans	
113552_Road_1.pdf	Road Plans	1-201
113552_Road_2.pdf	Road Plans	202-359
113552_Road_3.pdf	Road Plans	360-495
113552_Road_4.pdf	Road Plans	496-632

Using the “Project Signature Sheet” file to E-Sign and E-Seal projects (not the cover sheet)



Electronic Transition

ELECTRONIC COVER SHEETS:



MICHIGAN DEPARTMENT OF TRANSPORTATION PAUL C. AJEGBA, P.E. - DIRECTOR			
MILES: - LEFT-TURN PHASING AND HEMISPHERICAL VIDEO DETECTION INSTALLATION.			
DATE: 12/18/20	CS: 33082	DRAWING	SHEET
DESIGN UNIT: SIGNALS	JN: 210928A	TITLE SHEET	SECT 1
TSC: LANSING		TITLE 001	001

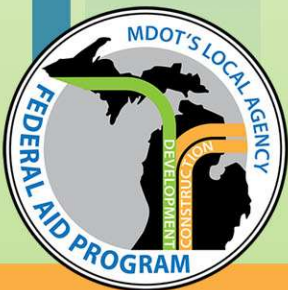
Remove the Signature Block from the Cover Sheet/Log Cover

OR

Add a Note in the Signature Block

“E-signed in the Project Signature File”

“See project Signature File”



*** A Signed and Sealed Cover Sheet or Project Signature Sheet is required prior to obligation.**



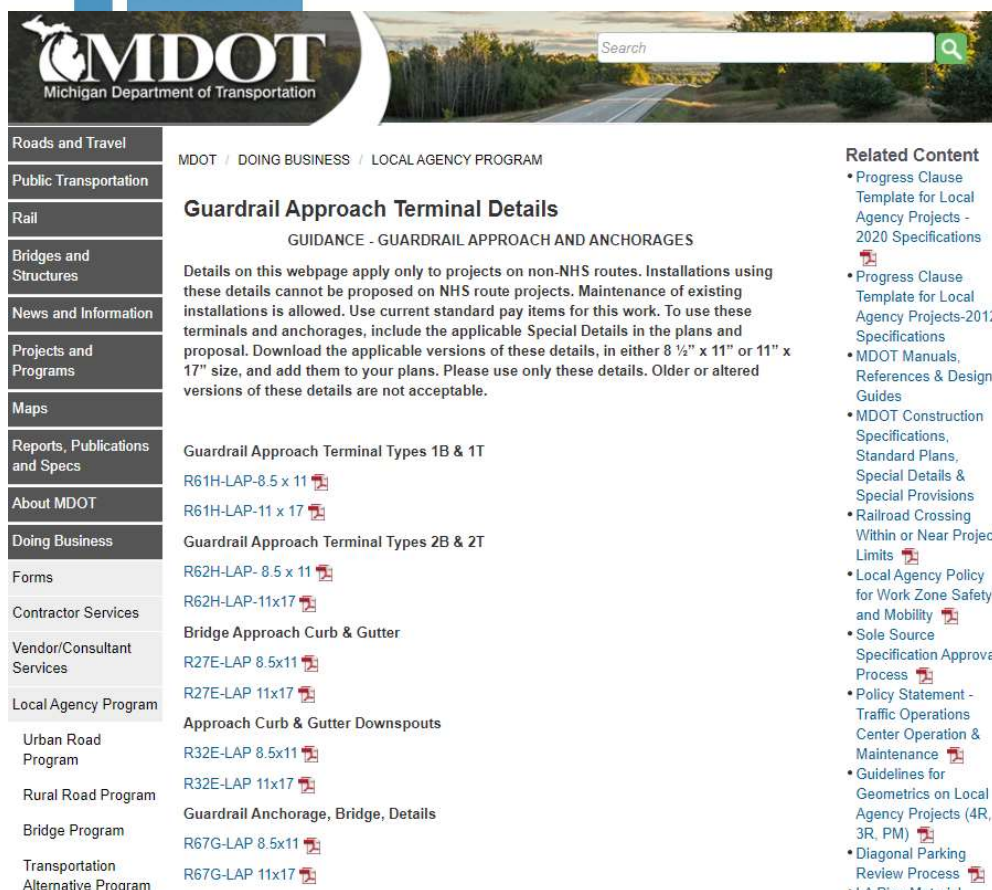
2020 Frequently Used Special Provisions (FUSPs)

- Some were incorporated into the 2020 Spec Book
- Some were retained as 2020 FUSPs

2012 FUSP ID	2020 FUSP ID	FUSP Title
12SP-102I-01	20SP-102A-01	Pass-Through Warranty Bonds
12SP-102J-01	20SP-102B-01	Multiple Warranties
12SP-102H-01	20SP-102C-01	Dissemination of Public Relations Information
12SP-104C-02	20SP-104A-01	Value Engineering Change Proposal
12SP-104D-05	20SP-104B-01	Dispute Review Board
12SP-104E-03	20SP-104C-01	Construction Documentation Management
12SP-104F-03	20SP-104D-01	Prevailing Wage and Labor Compliance System
12SP-105A-09	20SP-105A-01	Source of Steel and Iron (Buy America)
12SP-107C-01	20SP-107A-01	Sweeping
12SP-107D-02	20SP-107B-01	Migratory Bird Protection
12SP-107F-01	20SP-107C-01	E-Verify
12SP-107G-03	20SP-107D-01	Labor Compliance
12SP-107H-01	20SP-107E-01	Federal Aviation Administration Notification for Structure Work
12SP-107I-01	20SP-107F-01	Eastern Massasauga Rattlesnake
12SP-107L-01	20SP107G-01	Environmentally Sensitive Areas
12SP-108A-02	20SP-108A-01	Extension of Time and Increased Cost for Incentive Projects
12SP-108E-03	20SP-108B-01	Critical Path Method Schedule

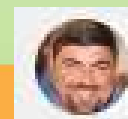


Guardrail Reminders



The screenshot shows the MDOT website interface. At the top left is the MDOT logo (Michigan Department of Transportation). A search bar is visible. A navigation menu on the left lists various categories: Roads and Travel, Public Transportation, Rail, Bridges and Structures, News and Information, Projects and Programs, Maps, Reports, Publications and Specs, About MDOT, Doing Business, Forms, Contractor Services, Vendor/Consultant Services, and Local Agency Program. The main content area is titled "Guardrail Approach Terminal Details" under the heading "GUIDANCE - GUARDRAIL APPROACH AND ANCHORAGES". It contains a paragraph of text: "Details on this webpage apply only to projects on non-NHS routes. Installations using these details cannot be proposed on NHS route projects. Maintenance of existing installations is allowed. Use current standard pay items for this work. To use these terminals and anchorages, include the applicable Special Details in the plans and proposal. Download the applicable versions of these details, in either 8 1/2" x 11" or 11" x 17" size, and add them to your plans. Please use only these details. Older or altered versions of these details are not acceptable." Below this text is a list of links for different terminal types: "Guardrail Approach Terminal Types 1B & 1T" with links for R61H-LAP-8.5 x 11 and R61H-LAP-11 x 17; "Guardrail Approach Terminal Types 2B & 2T" with links for R62H-LAP-8.5 x 11 and R62H-LAP-11x17; "Bridge Approach Curb & Gutter" with links for R27E-LAP 8.5x11 and R27E-LAP 11x17; "Approach Curb & Gutter Downspouts" with links for R32E-LAP 8.5x11 and R32E-LAP 11x17; and "Guardrail Anchorage, Bridge, Details" with links for R67G-LAP 8.5x11 and R67G-LAP 11x17. On the right side, there is a "Related Content" section listing various documents and guides such as "Progress Clause Template for Local Agency Projects - 2020 Specifications", "MDOT Manuals, References & Design Guides", and "Local Agency Policy for Work Zone Safety and Mobility".

- Special Details R-61-H, R-62-H, R-27-E, R-32-E and R-67-G have been removed
- Terminals on NHS routes must now be type 2M
- LAP will allow the use of type 1B, 1T, 2B, 2T on **non-NHS routes**.
- Projects must include the correct details, which can be found on the LAP website:
www.michigan.gov/mdotlap
(Select a LAP Unit) >> Design >> Guardrail Approach Terminal Details

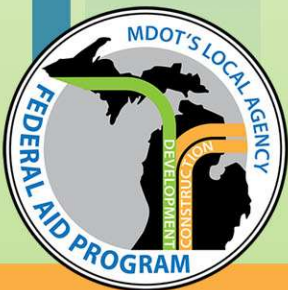


Guardrail Reminders

Other Items to Note:



- Detail 21's still have to be type B.
 - The end is a Type B departing terminal.
 - The MGS curve is for gradual curves only.
- Ending lengths are not all the same
 - Multiple suppliers of the Type 2M ending = different lengths (R-62-H)
 - Suggestion: Identify the station at the end of the guardrail run, but not the end of the terminal
 - If you need a specific ending length, you may need to utilize the sole source process



Guardrail Reminders

Other Items to Note:

MGS required on NHS routes and on Safety funded projects.

Height: Type MGS is 31" (ground to top of rail). Type B is 28".

- MGS guardrail to Type B - the height transition is included in the MGS guardrail pay item (R-60-J, sheet 10).
- Ending Replacements (2M terminal attached to existing Type B rail)
 - Height transition prior to the ending (included in the 2M ending pay item per 20SP-807F-01)
 - Don't forget to account for this length in your removal quantities (R-60-J sheet 11)

GCB:CT

3 of 3

20SP-807F-01
04-02-20

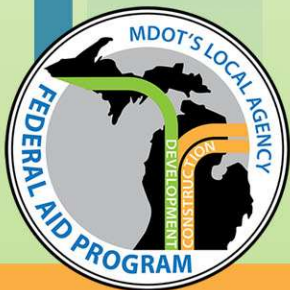
Guardrail Approach Terminal, Type 2M includes furnishing and installing a guardrail approach terminal meeting the requirements of this special provision.

Payment for **Guardrail Approach Terminal, Type 2M** includes all materials, labor, and equipment within the length of each terminal, as defined in subsections d.1, d.2, and d.3 of this special provision, and also includes payment for all materials, labor, and equipment required to construct a transition section, per Standard Plan R-60-Series, for connecting Guardrail Approach Terminal, Type 2M to guardrail Type B or Type T.

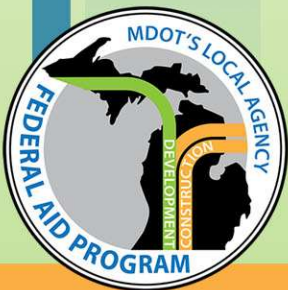


Pavement Markings – Line Width

- In support of connect and automated driving systems, MDOT has moved to 6” permanent and temporary lane lines
- For LAP Projects:
 - 4” lines will continue to be allowed
 - 4” pay items will remain
 - ... for now.
 - Local Agencies can convert to 6” if they desire



Sidewalk Pay Item Changes



Sidewalk Ramp is now: Curb Ramp

2012 Pay Items

8030033	Sft	Sidewalk Ramp, Conc, 3 inch
8030034	Sft	Sidewalk Ramp, Conc, 4 inch
8030036	Sft	Sidewalk Ramp, Conc, 6 inch
8030037	Sft	Sidewalk Ramp, Conc, 7 inch



2020 Pay Items

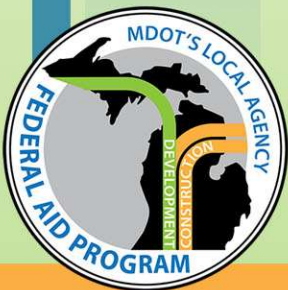
8032000	Sft	Curb Ramp, Conc, 3 inch
8032001	Sft	Curb Ramp, Conc, 4 inch
8032002	Sft	Curb Ramp, Conc, 6 inch
8032003	Sft	Curb Ramp, Conc, 7 inch



Curb Ramp Opening, Conc

8030030 Ft Curb Ramp Opening, Conc

- Stayed the same pay item name and code but is now incorporated into the 2020 Spec Book (no longer a FUSP).
- Includes the transitions to and from adjacent standard full-height curb and gutter cross section.
- Only need to pay separately for Curb and Gutter if you are replacing additional full-height curb and gutter.



Sidewalk Ramp is now: Curb Ramp

What all is included in the Curb Ramp pay item?

- How about the rolled curb along the ramp?
- What about the side flares?
- Landings?

Where does the Curb Ramp pay item stop and the regular Sidewalk pay item begin?



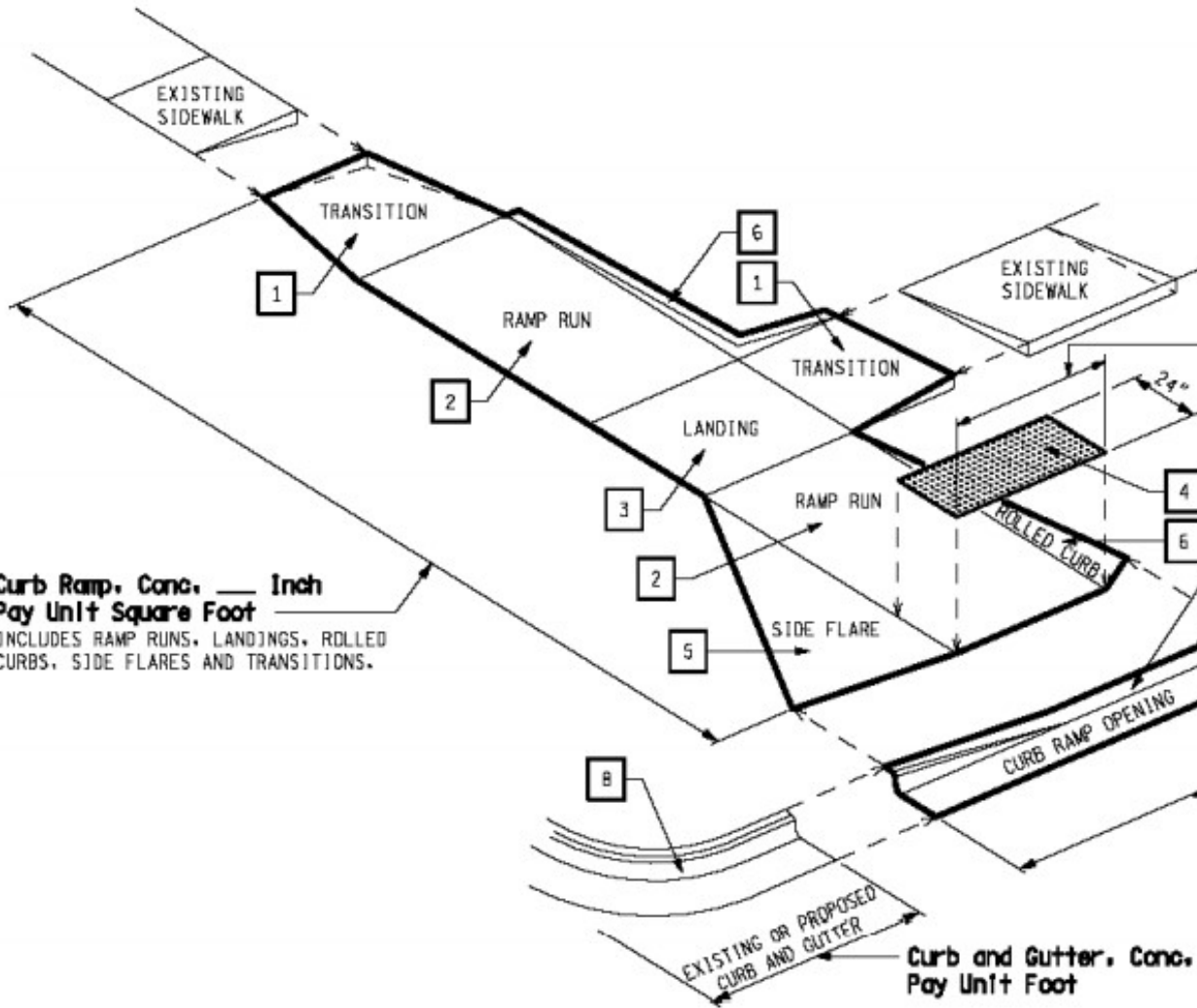
Sidewalk Ramp is now: Curb Ramp

Per 803.04 of the 2020 Spec Book:

C. Curb Ramp. The Engineer will measure Curb Ramp, Conc, ___ inch by the area of ramp and landing in place. Curb Ramp, Conc, ___ inch includes sidewalk sloped greater than the normal continuous sidewalk grades to meet the elevation of the curb opening or intermediate landing. Landing areas constructed and meeting the requirements of the MDOT Standard Plan R-28 series will be included in the Curb Ramp, Conc, ___ inch item.

The unit price for Curb Ramp, Conc, ___ inch includes the cost of landings, monolithic rolled curbs or side flares along the longitudinal edges of the ramp or landing, and transitions to existing sidewalk.





ID	ELEMENT	DESCRIPTION	PAY ITEM
1	TRANSITION	Slab used as needed to transition into existing dissimilar sidewalk cross section (width, depth, cross slopes, etc).	Curb Ramp, Conc. Inch
2	RAMP RUN	Slabs carrying ramp running slopes. "RAMP RUN" slabs may combine for a total distance (not including landings and transitions) of at least 15ft. between the back of curb and an existing sidewalk.	Curb Ramp, Conc. Inch
3	LANDINGS	Flat Slab required at the top of perpendicular ramp runs and the bottom of Type P, C and D ramps. See Standard Plan R-28-Series.	Curb Ramp, Conc. Inch
4	DETECTABLE WARNING SURFACE	Truncated dome surface across full width of ramp and 24" minimum in direction of travel.	Detectable Warning Surface
5	SIDE FLARE	1:10 maximum transition flares along walkable surface perpendicular to ramp runs.	Curb Ramp, Conc. Inch
6	ROLLED CURB	Roll ed curb (no maximum rate) along non-walkable surfaces adjacent to ramp runs. Also used for ramp infield grading retention up to 18 Inch height.	Curb Ramp, Conc. Inch
7	CURB RAMP OPENING	Flattened curb and gutter at ramp opening including transitions to and from full height curb and gutter.	Curb Ramp Opening, Conc
8	CURB AND GUTTER	Full height curb and gutter. Paid separately when called for on the plans.	Curb and Gutter, Conc, Det

**Curb Ramp, Conc. Inch
Pay Unit Square Foot**
INCLUDES RAMP RUNS, LANDINGS, ROLLED CURBS, SIDE FLARES AND TRANSITIONS.

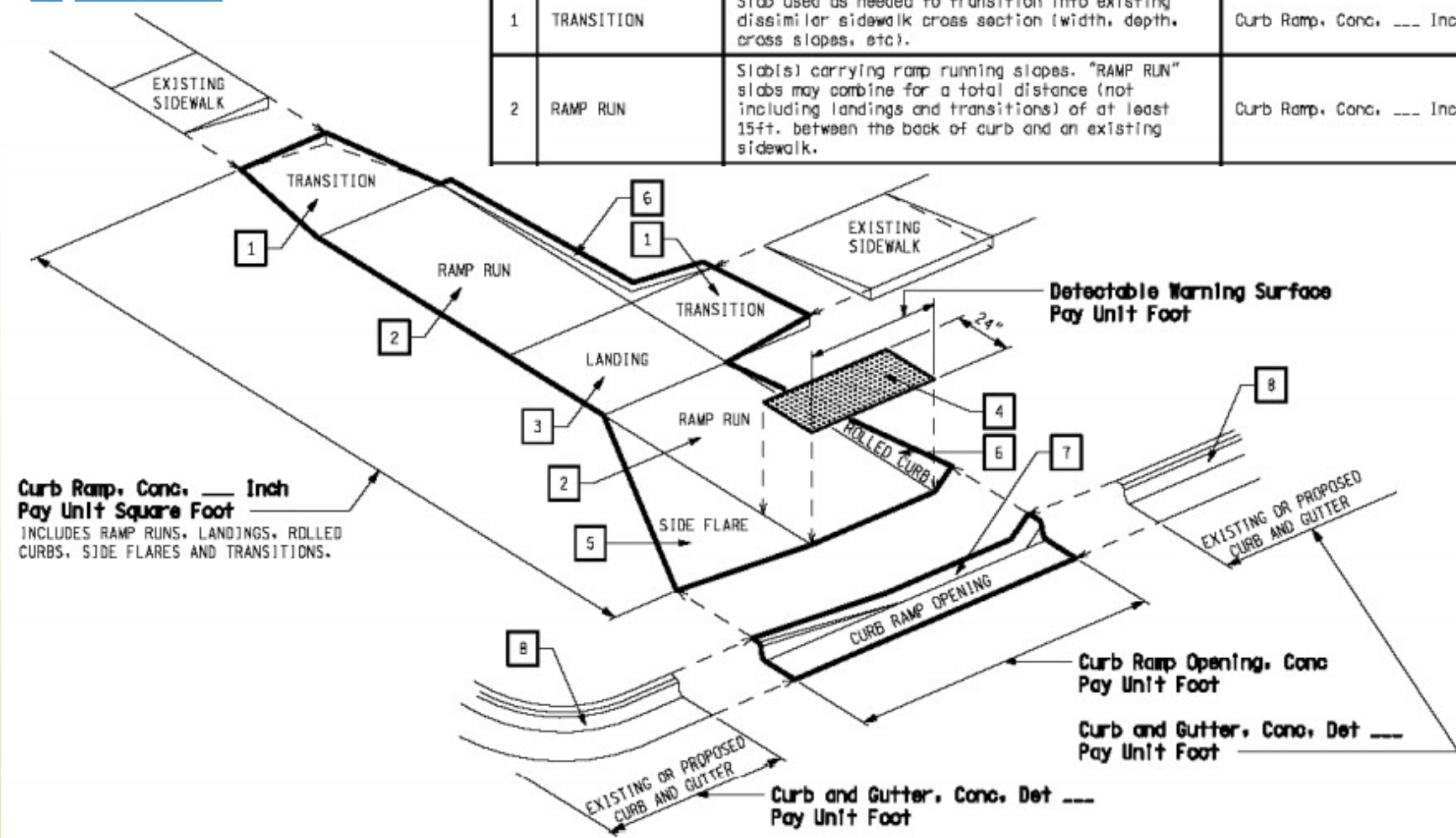
**Curb Ramp Opening, Conc
Pay Unit Foot**

**Curb and Gutter, Conc, Det
Pay Unit Foot**

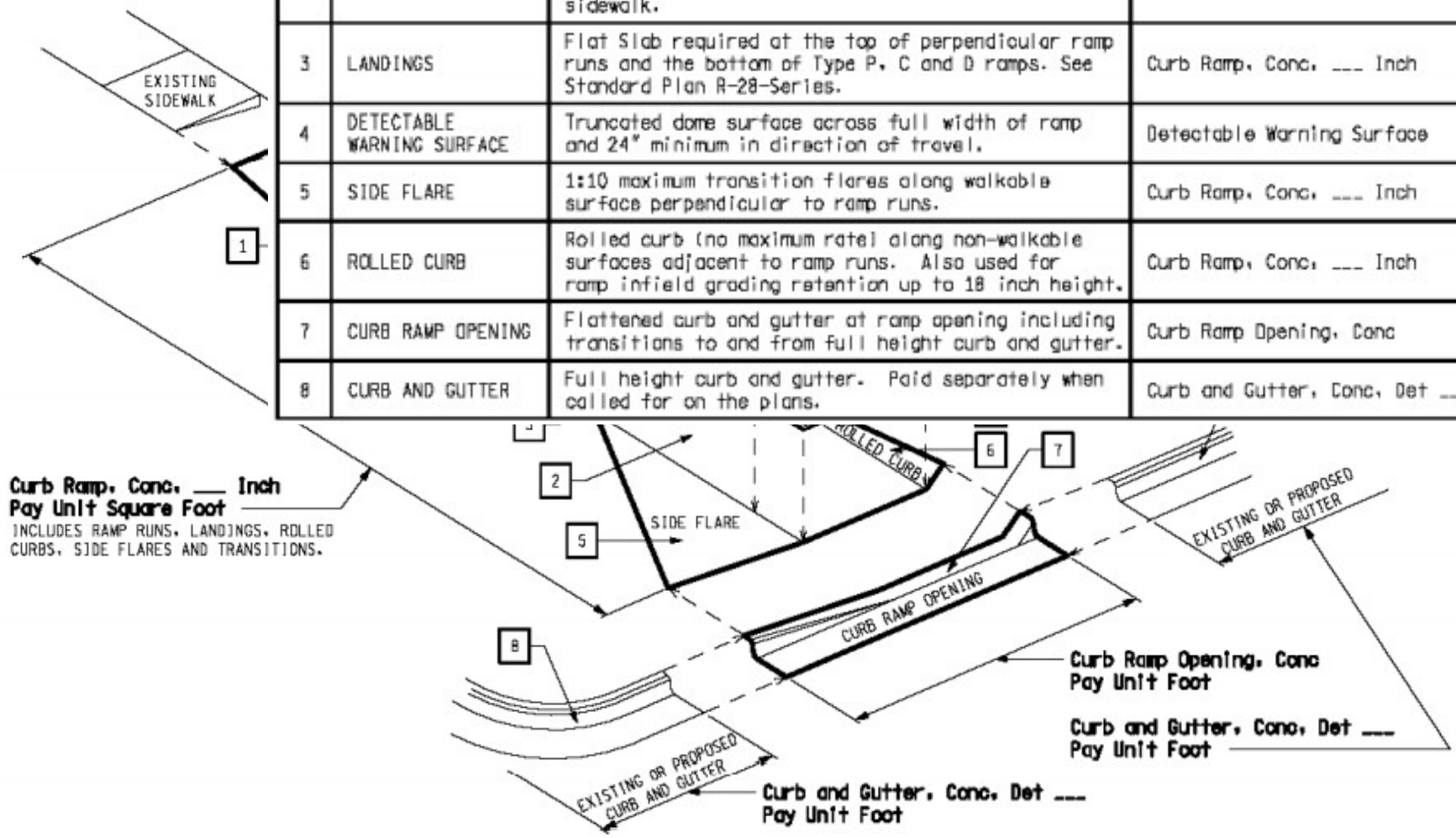
**Curb and Gutter, Conc, Det
Pay Unit Foot**



ID	ELEMENT	DESCRIPTION	PAY ITEM
1	TRANSITION	Slab used as needed to transition into existing dissimilar sidewalk cross section (width, depth, cross slopes, etc).	Curb Ramp, Conc. ___ Inch
2	RAMP RUN	Slab(s) carrying ramp running slopes. "RAMP RUN" slabs may combine for a total distance (not including landings and transitions) of at least 15ft. between the back of curb and an existing sidewalk.	Curb Ramp, Conc. ___ Inch



ID	ELEMENT	DESCRIPTION	PAY ITEM
1	TRANSITION	Slab used as needed to transition into existing dissimilar sidewalk cross section (width, depth, cross slopes, etc).	Curb Ramp, Conc. ___ Inch
2	RAMP RUN	Slab(s) carrying ramp running slopes. "RAMP RUN" slabs may combine for a total distance (not including landings and transitions) of at least 15ft. between the back of curb and an existing sidewalk.	Curb Ramp, Conc. ___ Inch
3	LANDINGS	Flat Slab required at the top of perpendicular ramp runs and the bottom of Type P, C and D ramps. See Standard Plan R-28-Series.	Curb Ramp, Conc. ___ Inch
4	DETECTABLE WARNING SURFACE	Truncated dome surface across full width of ramp and 24" minimum in direction of travel.	Detectable Warning Surface
5	SIDE FLARE	1:10 maximum transition flares along walkable surface perpendicular to ramp runs.	Curb Ramp, Conc. ___ Inch
6	ROLLED CURB	Rolled curb (no maximum rate) along non-walkable surfaces adjacent to ramp runs. Also used for ramp infield grading retention up to 18 inch height.	Curb Ramp, Conc. ___ Inch
7	CURB RAMP OPENING	Flattened curb and gutter at ramp opening including transitions to and from full height curb and gutter.	Curb Ramp Opening, Conc
8	CURB AND GUTTER	Full height curb and gutter. Paid separately when called for on the plans.	Curb and Gutter, Conc, Det ___



Other Updates:

FY23 Safety Call for Projects

- FY23 Applications are being reviewed and scored. Anticipate notice of award around Labor Day.

Updated Long Lead Time Items

- Guardrail Steel – 8 weeks
- Bridge Beams and Girders- 25 weeks
- Mast Arms/Strain Poles – 25 weeks
- Cabinets, Detection, ITS – 8 to ? weeks
- Signposts and Delineators – 21 to 35 weeks



Other Updates:

Short Range Communication Roadside Units

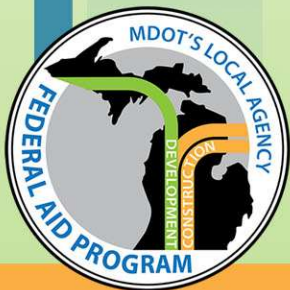
- MDOT is pulling RSUs from all on-going and future projects until they can determine how to address the new rules from the FCC regarding repurposing a portion of the 5.9 GHz band.

MDOT Telecommuting/Covid Policies

- In Person Meetings Allowed
- Site Visits Allowed
- Some Staff in Office

WORK CLOTHES

**NOW THAT IS SOMETHING I
HAVEN'T WORN IN A LONG TIME**



Updated Real Estate LPA Guidance Webpage (Feb 2021)



Having trouble viewing this email? [View it as a Web page.](#)

 SHARE

There have been updates to the Real Estate LPA Guidance webpage.

Link: https://www.michigan.gov/mdot/0,4616,7-151-9623_26662-516980--00.html

1. A **Water Service Replacement Quick Reference** guide has been added. Water service projects only need to be certified if they are being constructed under the MDOT Job Number.

2. Forms were added, updated, and removed. Below is a list of the changes:

- **Water Service Replacement Certification (NEW)** – Used for water service line replacement projects constructed under MDOT's job number.
- **Consent (NEW)** Example form used for acquisition of temporary rights.
- **Donation (UPDATED)** Minor changes.
- **Donation in Exchange for Features or Services (NEW)** Form used to donate property rights. The form shows property owner the cost of the feature the LPA is improving compared to the just compensation being offered (driveway grading costs \$3,000, just compensation is \$500). Property owner still must be offered just compensation prior to donation being discussed. The cost of the feature/service must have supporting documentation.
- **Good Faith Offer Letter (2 NEW)** Two letters have been provided depending on if acquisition is being acquired under the threat of condemnation or without the threat of condemnation.
- **Memorandum or Negotiations (NEW)** Log of negotiations.
- **Release of Damages (NEW)** Used for payment beyond just compensation or costs associated with damages to landscaping, etc.
- **Waiver of appraisal (REMOVED)** – This form is only used by MDOT, not LPA's

Contact: Kem Hoopingarner, MDOT-LPA@michigan.gov

[Updated Real Estate LPA Guidance Webpage LINK](#)



LPA Real Estate continued . . .



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There has been multiple updates to the LPA Real Estate [Guidance Page](#) in the last six months. Please make sure you are aware of the newest policies before starting your ROW projects. The following has been added to the LPA Real Estate Guidance Page in the last two weeks:

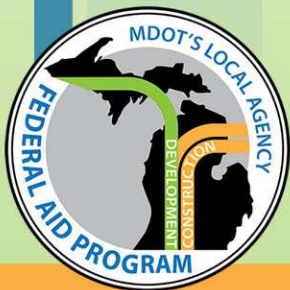
- Updated: Procedure Manual - Many updates, three significant updates are:
 - Governmental entities participating or partnering (contributing funding to project) in a non-motorized path project, may donate property rights to the LPA without need for an offer of just compensation. The entity must be participating in the project and must execute a donation form acknowledging they are aware they have the right to just compensation; however, have chosen to donate the rights. This requires written **pre-approval** by the MDOT Real Estate Coordinator.
 - Waiver Valuations - There are 3 approved methods for waiver valuations, person preparing waiver valuation must have sufficient understanding of the local real estate market.
 - Waiver valuation may be used for **government owned property** with value over \$10,000 up to \$25,000. This requires written **pre-approval** from the MDOT Real Estate Coordinator and written acknowledgement and agreement (Waiver of Appraisal) from the governmental entity that they:
 - Have been informed and understand that they have the right to receive just compensation.
 - Have been given the opportunity to review the waiver valuation used to establish the just compensation.
 - Have been informed and understand they have the right to an appraisal.
 - Have been informed and understand that by signing the acknowledgement they are waiving their right to an appraisal.
 - That they will execute the necessary conveyance instruments upon presentation by LPA.

- Updated: Quick Reference Guide
- Updated: Parcel Checklist
- New: Waiver Valuation Guide - 3 Approved Methods
- New: Project Spreadsheet Example
- New example forms:
 - Sidewalk Easement
 - Drainage Easement
 - Highway Easement
 - Waiver of Appraisal - Government to Government transactions between \$10,000-\$25,000 (see update to Procedure Manual above).

Reminder upcoming training:

- 4/20/21 - Waiver Valuation Webinar - Michigan's Local Technical Assistance Program - [REGISTER](#)
- 5/6/21 - Acquisition Package for Certification - International Right of Way Association

Direct questions related to this advisory may be directed to staff at: MDOT-LPA@michigan.gov



LPA Real Estate continued . . .



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The LPA Real Estate Guidance Website ([LINK](#)) has been updated for the following:

Administrative Settlement (Sample Forms): If the settled amount is more than the offered amount, the Agency must draft an Administrative Settlement Approval document explaining the reasoning for the settlement. Reasoning can include condemnation costs; however, condemnation costs should not be the only reason for the settlement. This document must be signed by an authorized Agency staff member.

Waiver Valuation Certification (Documents & Guidance): This form has been updated to be used for all three types of Waiver Valuations (Market Study, BPO, or Assessor's Sales Report). This replaces the three different Waiver Valuation Certifications.

Temporary Relocation Procedure Approval: FHWA has approved MDOT's Relocation Emergency Relief Program effective June 15, 2021 – June 2, 2023. This is a temporary procedure which provides emergency relief in situations where the Agency is having difficulty locating, and displacees are having difficulty securing, replacement housing. Please see Real Estate Bulletin below for direction.

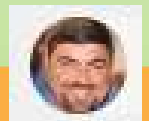
Procedure Manual Updates: There have been multiple updates to the LPA Section of the Real Estate Manual for clarification and the following:

- **9.3.1 File Documentation** - Added to the list of documents required and clarified items/language.
- **9.6 Acquisition of Public Property** - Governmental entities partnering/contributing to a transportation project may donate without the need for an offer of just compensation. This was changed from non-motorized paths only to transportation project. Needed documentation is addressed in the procedure.
- **9.7 Acquisition of Non-Motorized Paths** - Privately owned utility corridors has been added. Documentation and agreement requirements are addressed.
- **9.8 Right of Entry & Possession and Use Agreement** - Clarifies steps needed to use a ROE or PUA and restrictions.

Direct questions related to this advisory may be directed to staff at: MDOT-LPA@Michigan.gov

You are subscribed to Real Estate Updates for Local Public Agencies for Michigan DOT. This information has recently been updated, and is now available.

- [Real Estate Bulletin 2021-06-17-21.pdf](#)



Design Advisory – Property Acquisitions



Having trouble viewing this email? [View it as a Web page.](#)

MDOT Local Agency Programs (LAP) posted an advisory on its website, reminding local agencies and its consultants of the process for documenting property acquisition needed for construction projects using federal or state transportation funding..

The advisory is posted on the LAP website (www.michigan.gov/mdotlap), in the section headed "Local Agency Programs (LAP) Advisories" that is reached by clicking on the link "Local Agency Programs (LAP) Advisories" on the opening page of the web site.

The link to the document is titled [03-31-2021 - Property Acquisition Updates](#).

Direct questions related to this advisory to MDOT Real Estate staff at: MDOT-LPA@Michigan.gov

Local Agency Program Information Advisory

LAP 2021-02
March 31, 2021

From Tracie Leix, Local Agency Program Engineer

Property Acquisition - Program Application Attachment A & B Submittals

This guidance applies to Local Agency transportation projects, funded all or in part with federal and state transportation funds, which require property acquisition for successful completion of the project.

If property acquisition documentation submitted to MDOT does not meet the guidance requirements, then funds cannot be obligated, **which may delay the project from bid advertisement and letting.** Local agencies and their consultants that acquire property should do so according to the following procedure.

MDOT
Development Services Division
Local Agency Program
PO Box 30050
Lansing, MI 48909
Phone/517-373-2300
Fax/517-335-2209
www.michigan.gov/mdotlap



[Design Advisory 2021-03-31 LINK](#)

Recent Updates to Work Zone Safety and Mobility Manual



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 SHARE

Everyone,

The Work Zone Safety and Mobility Manual has been updated. A detailed list of the changes in noted below.

[https://www.michigan.gov/documents/mdot/MDOT Work Zone Safety and Mobility Manual-December 20202 709728 7.pdf](https://www.michigan.gov/documents/mdot/MDOT_Work_Zone_Safety_and_Mobility_Manual-December_20202_709728_7.pdf)

Updated index
Updated acronyms
Links updated
Minor corrections and clarifications.



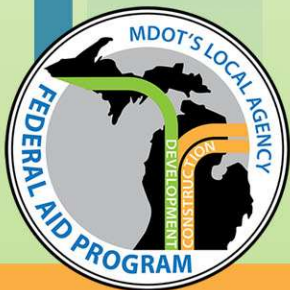
[MDOT Work Zone Safety and Mobility Manual LINK](#)



LAP

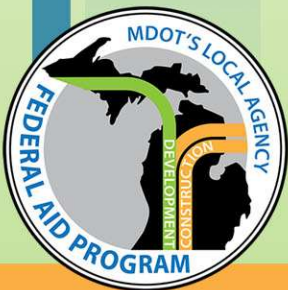
Construction

Kelly Crannell

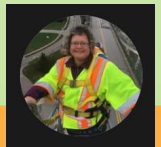


MDOT Construction Manual

- Local Agency Program section updated
 - Responsible Charge defined
 - Construction Engineering Requirements
 - links to ‘LAP Project Considerations’



[LAP Section of MDOT Construction Manual - LINK](#)

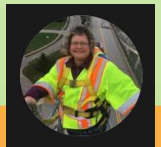


Construction Manual updates continued

- Project Close Out Process Improvements
 - Final Project Review Procedures (FPR)
 - Interim File Review
 - Escalation of Accountability
 - Certified Engineer Program – opening to Consultants
 - Letter to file procedures

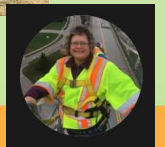


[CM Division 1 Supplemental - File Closeout LINKS](#)



Fabrication Inspection

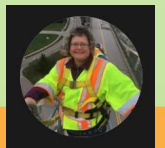
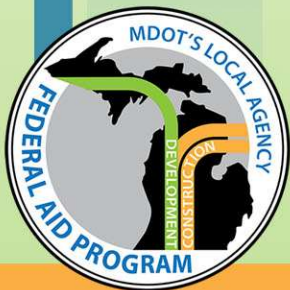
- CFR requirement
 - Cannot write out of proposal with a unique SP
 - Cannot omit during construction
- MDOT Manual Updates
 - Material Quality Assurance Procedures (MQAP)
 - Structural Fabrication Quality Manual (SFQM) - NEW



Material Shortages

FHWA has not declared any national material shortages

Questions about contractor claims?
Please contact your MDOT Responsible Charge/Designated Rep assigned from the overseeing TSC to discuss



HMA Testing

Per 12SP-501J

A. Composition of HMA Mixtures. **2012 Spec Book**

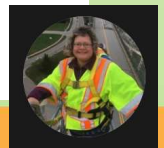
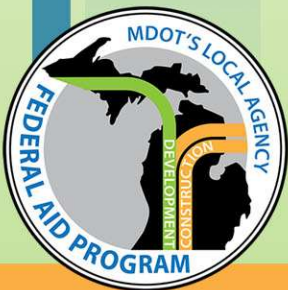
1. **Mix Design.** Develop an HMA mix design in accordance with the *HMA Production Manual* and submit to the Department. The Department will evaluate the design in accordance with Section 1 of the *HMA Production Manual*, "Procedures for HMA Mix Design Processing."

Provide written certification that the materials in the mix design are from the same source and meet the material properties in the mix design or the Department-approved JMF. Ensure that all JMF adjustments are in accordance with the *HMA Production Manual*.

The Contractor may use mix designs approved by the Department on other projects, if approved by the Engineer. Provide combined aggregate blends meeting the properties specified in section 902. Provide a mix design that meets the requirements of Table 501-1, Table 501-2, and Table 501-3 as applied to combined aggregate blends.

For mix design purposes, top and leveling courses are the mix layers within 4 inches of the surface. The base course consists of the layers below 4 inches from the surface. For mix layers within the 4-inch threshold, if less than 25 percent of the mix layer is within 4 inches of the surface, the mix layer is a base course.

For projects that specify a mix type E03, the Contractor may use a mix type LVSP.



MICHIGAN
DEPARTMENT OF TRANSPORTATION

SPECIAL PROVISION
FOR
ACCEPTANCE OF HOT MIX ASPHALT MIXTURE ON LOCAL AGENCY PROJECTS

CFS:KPK

1 of 7

APPR:CJB:JWB:07-05-16
FHWA:APPR:07-05-16

a. Description. This special provision provides sampling and testing requirements for local agency projects using the roller method and the nuclear density gauge testing. Provide the hot mix asphalt (HMA) mixture in accordance with the requirements of the standard specifications, except where modified herein.

b. Materials. Provide aggregates, mineral filler (if required), and asphalt binder to produce a mixture proportioned within the master gradation limits shown in the contract, and meeting the uniformity tolerance limits in Table 1.

Table 1: Uniformity Tolerance Limits for HMA Mixtures

Parameter		Top and Leveling Course		Base Course		
Number	Description	Range 1 (a)	Range 2	Range 1 (a)	Range 2	
1	% Binder Content	-0.30 to +0.40	±0.50	-0.30 to +0.40	±0.50	
2	% Passing	# 8 and Larger Sieves	±5.0	±8.0	±7.0	±9.0
		# 30 Sieve	±4.0	±6.0	±6.0	±9.0
		# 200 Sieve	±1.0	±2.0	±2.0	±3.0
3	Crushed Particle Content (b)	Below 10%	Below 15%	Below 10%	Below 15%	

a. This range allows for normal mixture and testing variations. The mixture must be proportioned to test as closely as possible to the Job-Mix-Formula (JMF).
b. Deviation from JMF.

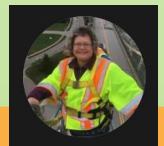
Parameter number 2 as shown in Table 1 is aggregate gradation. Each sieve will be evaluated on one of the three gradation tolerance categories. If more than one sieve is exceeding Range 1 or Range 2 tolerances, only the one with the largest exceedance will be counted as the gradation parameter.

The master gradation should be maintained throughout production; however, price adjustments will be based on Table 1. Aggregates which are to be used in plant-mixed HMA mixtures must not contain topsoil, clay, or loam.

c. Construction. Submit a Mix Design and a JMF to the Engineer. Do not begin production and placement of the HMA until receipt of the Engineer's approval of the JMF. Maintain the binder content, aggregate gradation, and the crushed particle content of the HMA mixture within the Range 1 uniformity tolerance limits in Table 1. For mixtures meeting the definition of top or leveling course, field regress air void content to 3.5 percent with liquid asphalt cement unless

12SP-501J HMA Testing (20SP-501I)

[12SP-501J-05 LINK](#)



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specified otherwise on HMA application estimate. For mixtures meeting the definition of base course, field regress air void content to 3.0 percent with liquid asphalt cement unless specified otherwise on HMA application estimate.

Ensure all persons performing Quality Control (QC) and Quality Assurance (QA) HMA field sampling are "Local Agency HMA Sampling Qualified" samplers. At the Pre-Production or Pre-Construction meeting, the Engineer will determine the method of sampling to be used. Ensure all sampling is done in accordance with *MTM 313 (Sampling HMA Paving Mixtures)* or *MTM 324 (Sampling HMA Paving Mixtures Behind the Paver)*. Samples are to be taken from separate hauling loads.

For production/mainline type paving, obtain a minimum of two samples, each being 20,000 grams, each day of production, for each mix type. The Engineer will sample and maintain possession of the sample. Sampling from the paver hopper is prohibited. Each sample will be divided into two 10,000 gram parts with one part being for initial testing and the other part being held for possible dispute resolution testing. Obtain a minimum of three samples for each mix type regardless of the number of days of production.

Obtain samples that are representative of the day's paving. Sample collection is to be spaced throughout the planned tonnage. One sample will be obtained in the first half of the tonnage and the second sample will be obtained in the second half of the tonnage. If planned paving is reduced or suspended, when paving resumes, the remaining sampling must be representative of the original intended sampling timing.

Ensure all persons performing testing are Bit Level One certified or Bit QA/QC Technician certified.

Ensure daily test samples are obtained, except, if the first test results show that the HMA mixture is in specification, the Engineer has the option of not testing additional samples from that day.

At the Pre-Production or Pre-Construction meeting, the Engineer and Contractor will collectively determine the test method for measuring asphalt content (AC) using *MTM 319 (Determination of Asphalt Content from Asphalt Paving Mixtures by the Ignition Method)* or *MTM 325 (Quantitative Extraction of Bitumen from HMA Paving Mixtures)*. Back calculation will not be allowed for determining asphalt content.

Ensure all labs performing local agency acceptance testing are qualified labs per the *HMA Production Manual* and participate in the MDOT round robin process, or they must be *AASHTO Materials Reference Laboratory (AMRL)* accredited for *AASHTO T 30* or *T 27*, and *AASHTO T 164* or *T 308*. Ensure on non-National Highway System (NHS) routes, Contractor labs are made available, and may be used, but they must be qualified labs as previously stated. Contractor labs may not be used on NHS routes. Material acceptance testing will be completed by the Engineer within 14 calendar days, except holidays and Sundays, for projects with less than 5,000 tons (plan quantity) of HMA and within 7 calendars days, except holidays and Sundays, for projects with 5,000 tons (plan quantity) or more of HMA, after the Engineer has obtained the samples. QA test results will be provided to the Contractor after the Engineer receives the QC test results. Failure on the part of the Engineer or the laboratory to provide Quality Assurance test results within the specified time frame does not relieve the Contractor of their responsibility to provide an asphalt

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The correlation procedure for ignition oven will be established as follows. Asphalt binder content based on ignition method from *MTM 319*. Gradation (*ASTM D 5444*) and Crushed particle content (*MTM 117*) based on aggregate from *MTM 319*. The incineration temperature will be established at the Pre-Production Meeting. The Contractor will provide a laboratory mixture sample to the acceptance laboratory to establish the correction factor for each mix. Ensure this sample is provided to the Engineer a minimum of 14 calendar days prior to production.

For production/mainline type paving, the mixture may be accepted by visual inspection up to a quantity of 500 tons per mixture type, per project (not per day). For non-production type paving defined as driveways, approaches, and patching, visual inspection may be allowed regardless of the tonnage.

The mixture will be considered out-of-specification, as determined by the acceptance tests, if for any one mixture, two consecutive tests per parameter, (for Parameter 2, two consecutive aggregate gradations on one sieve) are outside Range 1 or Range 2 tolerance limits. If a parameter is outside of Range 1 tolerance limits and the second consecutive test shows that the parameter is outside of Range 2, then it will be considered to be a Range 1 out-of-specification. Consecutive refers to the production order and not necessarily the testing order. Out-of-specification mixtures are subject to a price adjustment per the Measurement and Payment section of this special provision.

Contractor operations will be suspended when the mixture is determined to be out-of-specification, but contract time will continue to run. The Engineer may issue a Notice of Non-Compliance with Contract Requirements (Form 1165), if the Contractor has not suspended operations and taken corrective action. Submit a revised JMF or proposed alterations to the plant and/or materials to achieve the JMF to the Engineer. Effects on the Aggregate Wear Index (AWI) and mix design properties will be taken into consideration. Production and placement cannot resume until receipt of the Engineer's approval to proceed.

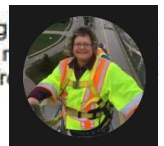
Pavement in-place density will be measured using one of two approved methods. The method used for measuring in-place density will be agreed upon at a pre-production or pre-construction meeting.

Pavement in-place density tests will be completed by the Engineer during paving operations and prior to traffic staging changes. Pavement in-place density acceptance testing will be completed by the Engineer prior to paving of subsequent lifts and being open to traffic.

Option 1 – Direct Density Method

Use of a nuclear density gauge requires measuring the pavement density using the JMF for the density control target. The required in-place density of the HMA is 92.0 to 98.0 percent of the density control target. Nuclear density testing and frequency will be in accordance with the *MDOT Density Testing and Inspection Manual*.

Option 2 – Roller Method



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Use of the density gauge requires establishing a rolling pattern that will achieve the required in-place density. The Engineer will measure pavement density with a density gauge using the Gmm from the JMF for the density control target.

Use of the Roller Method requires developing and establishing density frequency curves, and meeting the requirements of Table 2. A density frequency curve is defined as the measurement and documentation of each pass of the finished roller until the in-place density results indicate a decrease in value. The previous recording will be deemed the optimal density. The Contractor is responsible for establishing and documenting an initial or QC rolling pattern that achieves the optimal in-place density. When the density frequency curve is used, the Engineer will run and document the density frequency curve for each half day of production to determine the number of passes to achieve the maximum density. Table 5, located at the end of this special provision, can be used as an aid in developing the density frequency curve. The Engineer will perform density tests using an approved nuclear or non-nuclear gauge per the manufacturer's recommended procedures.

Table 2: Minimum Number of Rollers Recommended Based on Placement Rate

Average Laydown Rate, Square Yards per Hour	Number of Rollers Required (a)	
	Compaction	Finish
Less than 600	1	1 (b)
601 - 1200	1	1
1201 - 2400	2	1
2401 - 3600	3	1
3601 and More	4	1

a. Number of rollers may increase based on density frequency curve.
b. The compaction roller may be used as the finish roller also.

After placement, roll the HMA mixture as soon after placement as the roller is able to bear without undue displacement or cracking. Start rolling longitudinally at the sides of the lanes and proceed toward the center of the pavement, overlapping on successive trips by at least half the width of the drum. Ensure each required roller is 8 tons minimum in weight unless otherwise approved by the Engineer.

Ensure the initial breakdown roller is capable of vibratory compaction and is a maximum of 500 feet behind the paving operations. The maximum allowable speed of each roller is 3 miles per hour (mph) or 4.5 feet per second. Ensure all compaction rollers complete a minimum of two complete rolling cycles prior to the mat temperature cooling to 180 degrees Fahrenheit (F). Continue finish rolling until all roller marks are eliminated and no further compaction is possible. The Engineer will verify and document that the roller pattern has been adhered to. The Engineer can stop production when the roller pattern is not adhered to.

d. Measurement and Payment. The completed work, as described, will be measured and paid for using applicable pay items as described in subsection 501.04 of the Standard Specifications for Construction, or the contract, except as modified below.

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Base Price. Price established by the Department to be used in calculating incentives and adjustments to pay items and shown in the contract.

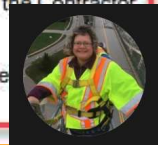
If acceptance tests, as described in section c. of this special provision, show that a Table 1 mixture parameter exceeds the Range 1, but not the Range 2, tolerance limits, that mixture parameter will be subject to a 10 percent penalty. The 10 percent penalty will be assessed based on the acceptance tests only unless the Contractor requests that the 10,000 gram sample part retained for possible dispute resolution testing be tested. The Contractor has 4 calendar days from receipt of the acceptance test results to notify the Engineer, in writing, that dispute resolution testing is requested. The Contractor's QC test results for the corresponding QA test results must result in an overall payment greater than QA test results otherwise the QA tests will not be allowed to be disputed. The Engineer has 4 calendar days to send the dispute resolution sample to the lab once dispute resolution testing is requested. The dispute resolution sample will be sent to an independent lab selected by the Local Agency, and the resultant dispute test results will be used to determine the penalty per parameter, if any. Ensure the independent lab is a MDOT QA/QC qualified lab or an AMRL HMA qualified lab. The independent lab must not have conflicts of interest with the Contractor or Local Agency. If the dispute testing results show that the mixture parameter is out-of-specification, the Contractor will pay for the cost of the dispute resolution testing and the contract base price for the material will be adjusted, based on all test result parameters from the dispute tests, as shown in Table 3 and Table 4. If the dispute test results do not confirm the mixture parameter is out-of-specification, then the Local Agency will pay for the cost of the dispute resolution testing and no price adjustment is required.

If acceptance tests, as described in section c. of this special provision, show that a Table 1 mixture parameter exceeds the Range 2 tolerance limits, the 10,000 gram sample part retained for possible dispute resolution testing will be sent, within 4 calendar days, to the MDOT Central Laboratory for further testing. The MDOT Central Laboratory's test results will be used to determine the penalty per mixture parameter, if any. If the MDOT Central Laboratory's results do not confirm the mixture parameter is out-of-specification, then no price adjustment is required. If the MDOT Central Laboratory's results show that the mixture is out-of-specification and the Engineer approves leaving the out-of-specification mixture in place, the contract base price for the material will be adjusted, based on all parameters, as shown in Table 3 and Table 4.

In the case that the Contractor disputes the results of the test of the second sample obtained for a particular day of production, the test turn-around time frames given would apply to the second test and there would be no time frame on the first test.

The laboratory (MDOT Central Laboratory or independent lab) will complete all Dispute Resolution testing and return test results to the Engineer, who will provide them to the Contractor within 13 calendar days upon receiving the Dispute Resolution samples.

In all cases, when penalties are assessed, the penalty applies to each parameter, that is out of specification.



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Table 3: Penalty Per Parameter

Mixture Parameter out-of-Specification per Acceptance Tests	Mixture Parameter out-of-Specification per Dispute Resolution Test Lab	Price Adjustment per Parameter
NO	N/A	None
YES	NO	None
	YES	Outside Range 1 but not Range 2: decrease by 10% Outside Range 2: decrease by 25%

The quantity of material receiving a price adjustment is defined as the material produced from the time the first out-of-specification sample was taken until the time the sample leading to the first in-specification test was taken.

Each parameter of Table 1 is evaluated with the total price adjustment applied to the contract base price based on a sum of the two parameter penalties resulting in the highest total price adjustment as per Table 4. For example, if three parameters are out-of-specification, with two parameters outside Range 1 of Table 1 tolerance limits, but within Range 2 of Table 1 limits and one parameter outside of Range 2 of Table 1 tolerance limits and the Engineer approves leaving the mixture in place, the total price adjustment for that quantity of material is 35 percent.

Table 4: Calculating Total Price Adjustment

Cost Adjustment as a Sum of the Two Highest Parameter Penalties		
Number of Parameters Out-of-Specification	Range(s) Outside of Tolerance Limits of Table 1 per Parameter	Total Price Adjustment
One	Range 1	10%
	Range 2	25%
Two	Range 1 & Range 1	20%
	Range 1 & Range 2	35%
	Range 2 & Range 2	50%
Three	Range 1, Range 1 & Range 1	20%
	Range 1, Range 1 & Range 2	35%
	Range 1, Range 2 & Range 2	50%
	Range 2, Range 2 & Range 2	50%

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Table 5: Density Frequency Curve Development

Tested by: _____ Date/Time: _____

Route/Location:		Air Temp:
Control Section/Job Number:		Weather:
Mix Type:	Tonnage:	Gauge:
Producer:	Depth:	Gmm:

Roller #1 Type: _____

Pass No.	Density	Temperature	Comments
1			
2			
3			
4			
5			
6			
7			
8			
Optimum			

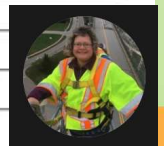
Roller #2 Type: _____

Pass No.	Density	Temperature	Comments
1			
2			
3			
4			
5			
6			
7			
8			
Optimum			

Roller #3 Type: _____

Pass No.	Density	Temperature	Comments
1			
2			
3			
4			
5			
6			
7			
8			
Optimum			

Summary: _____



MERS (Michigan Employment Reporting System)

Too many projects are not getting proper approvals!

Anticipating FHWA Audit

Should make best practice to check correctness of information on construction contract inquiry page

Bi-weekly Period Ending coding:

means reviewed

* means still needs review

MDOT 2134A Employment Reporting System - Google Chrome
mloginworker.michigan.gov/mdot-net/public/MERS/

Biweekly Data Entry

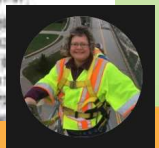
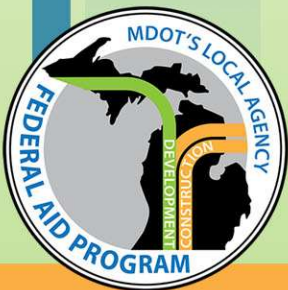
Prime Contractor: Ajax Paving Industries, Inc. (Vendor# 00580)

Contracts (with last submitted biweekly ending date): 37832-18847 (08/01/2025) - Marine and ramp rehabilitation including drainage and p...

Bi-Weekly Period Ending: 06/23/2025 - 07/04/2025 * # * = Submitted, # = Approved

Print 2134A Approved by Shari Bates On 07/10/2025

Subcontractor ID#	DBE/WBE, Self, Truckee, Supplier, or Service Provider	DBE/WBE?	Services/Work Classes	Total Subcontract Amount	DBE Commitment Amount	BiWeekly Value of Services Completed	Value of Work Completed to date	Deficiencies to-date (Bonds, Fees, holdbacks)	Deduction Comments	Actual Amount Paid to date
001	Action Traffic Maintenance, Inc.			396488.3		0.00	605192.00		5951.92	500140.10
002	Capital Benchmarking, Inc.	D+H		126488	127006	0.00	196551.81			196551.81
003	Diane Dukes, Inc.	D+H		11295	341818.83	2340.00	3840.00		36.40	3001.60
004	Martin J. Concrete, Inc.	DBE		287967.7	229644.7	0.00	261872.57			291872.57
005	Jonny Heintz Landscaping, LLC	D+H		67886	67886	0.00	0.00			0.00
006	L.J. Construction, Inc.			1070196.87		0.00	0.00		0.00	0.00
007	Poco Sales, Inc.			112525.5		7449.95	103328.92		1833.39	132305.53
008	Diane Dukes, Inc.	D+H		330059.94	341818.83	0.00	321075.36			321075.36
009	Advanced Underground Inspection, LLC	D+H		11078.36		0.00	10001.41		0.00	10001.41
010	C. A. Hall Co., Inc.			1348488.2		0.00	1376420.38		0.00	1376420.38
011	Central Asphalt, Inc.			804054.90		85179.13	1871192.23			1871192.23
012	Concrete Cutting & Breaking Co.			2198		0.00	0.00			0.00
013	Florence Cement Company			434074.3		0.00	364301.30		3643.02	36071
014	J. Ranch Electric, Inc.			48000		0.00	27375.00			27375.00
015	J. S. M. Excavation Company			981796.18		10701.34	1071080.18		0.00	1071080.18

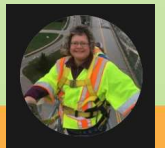


Prompt Payment

FHWA Focus Area

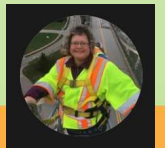
Remember to:

- submit all subcontracts to Contract Service Division for CAS entry (this is needed to populate MERS)
- verify payments have been paid to subcontractors **PRIOR TO PROCESSING ADDITIONAL ESTIMATES**, by checking and approving in MERS (1st & 2nd estimates ok to process on good faith – see 12SP-109A-03)
- Process Timely Contract Modifications



Progress Schedule

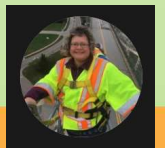
- Schedule submitted after award
- No interaction w/contractor prior to award
- Schedule format flexibility



Local Outreach/Training



- LAP Workshop for Locals (Spring 2022)
- LTAP Webinars
 - Office Tech 101
 - Inspection 101
 - 2020 Spec Book
- Technical Training schedule pending
 - MDOT is now outsourcing Certified Office Technician Training
 - \$600 charge for class this year
 - \$200 charge for recertification class



Questions?

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Kelly Crannell, P.E.
MDOT Local Agency Construction Engineer
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